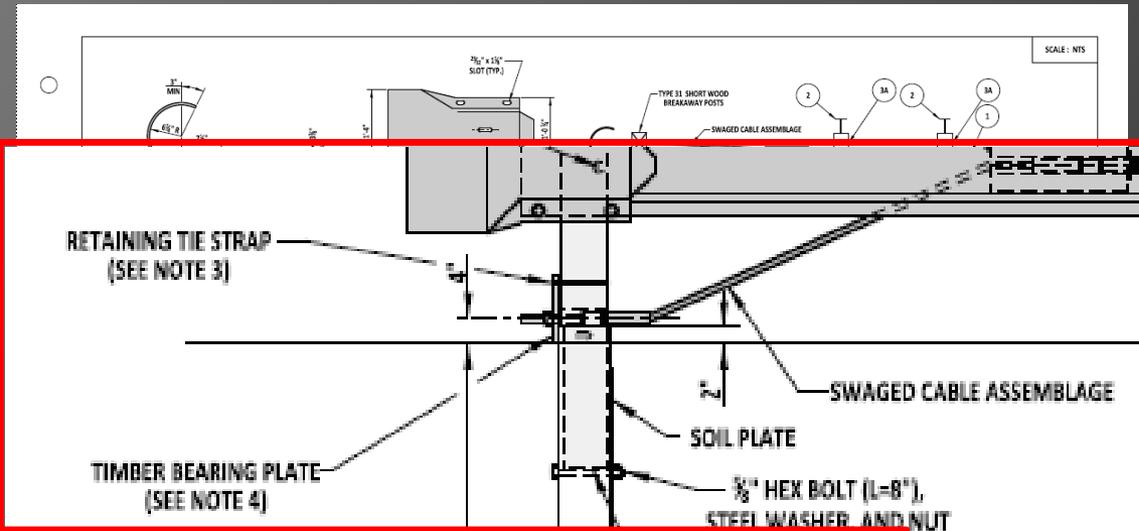


2012 STANDARD CONSTRUCTION DETAILS REVISION

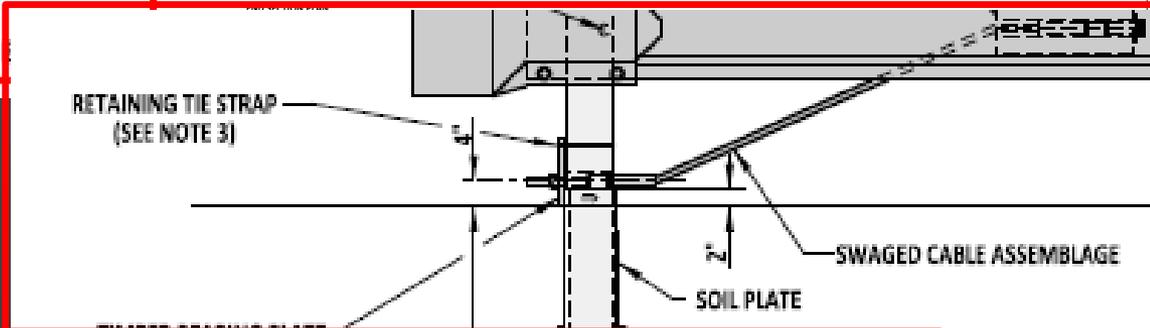
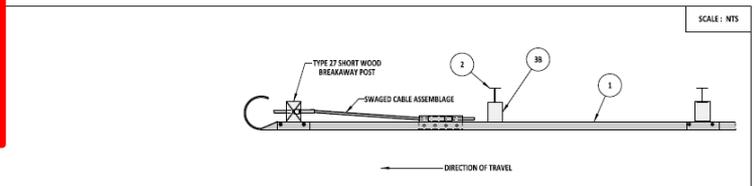
By: Billy Sweeney
DeIDOT – Quality Section

DETAIL B-4, SHEET 1 - END ANCHORAGE, TYPE 31

DETAIL B-19, SHEET 1 - END ANCHORAGE, TYPE 27



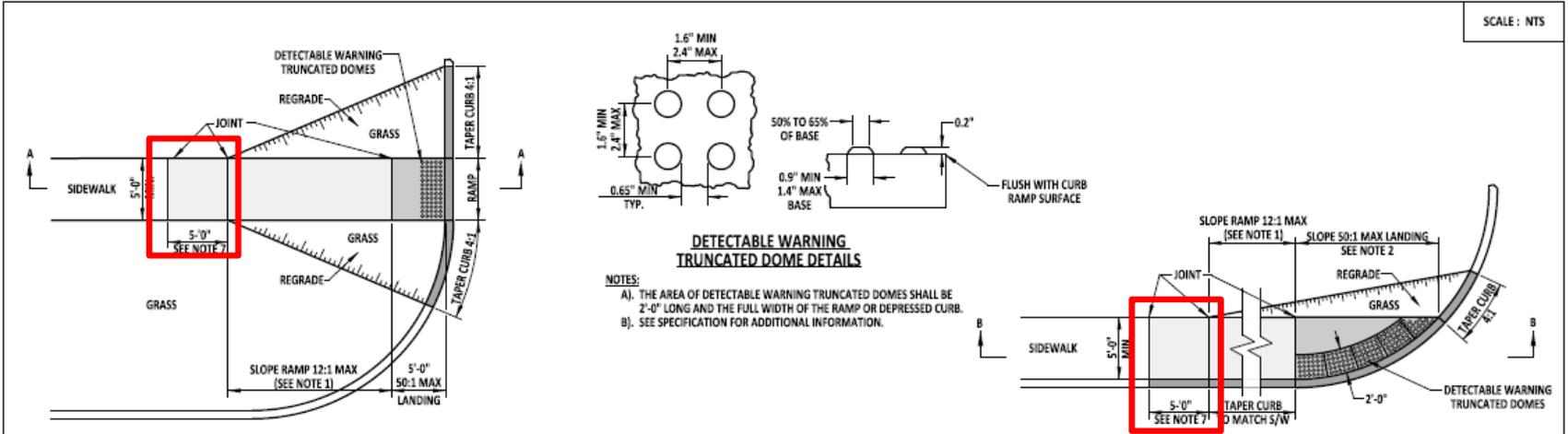
3/8" WIDE PLASTIC RETAINING TIE STRAP AROUND THE SHORT TIMBER BREAKAWAY POST AND TIMBER BEARING PLATE TO ENSURE THE PROPER ORIENTATION OF THE TIMBER BEARING PLATE.
 REFER TO DETAIL B-13, SHEET 8 OF 10 FOR PROPER TIMBER BEARING PLATE ORIENTATION.



- 3). PLACE A 3/8" WIDE PLASTIC RETAINING TIE STRAP AROUND THE SHORT TIMBER BREAKAWAY POST AND TIMBER BEARING PLATE TO ENSURE THE PROPER ORIENTATION OF THE TIMBER BEARING PLATE.
- 4). REFER TO DETAIL B-13, SHEET 8 OF 10 FOR PROPER TIMBER BEARING PLATE ORIENTATION.

DETAIL C-2, ALL SHEETS - CURB RAMPS

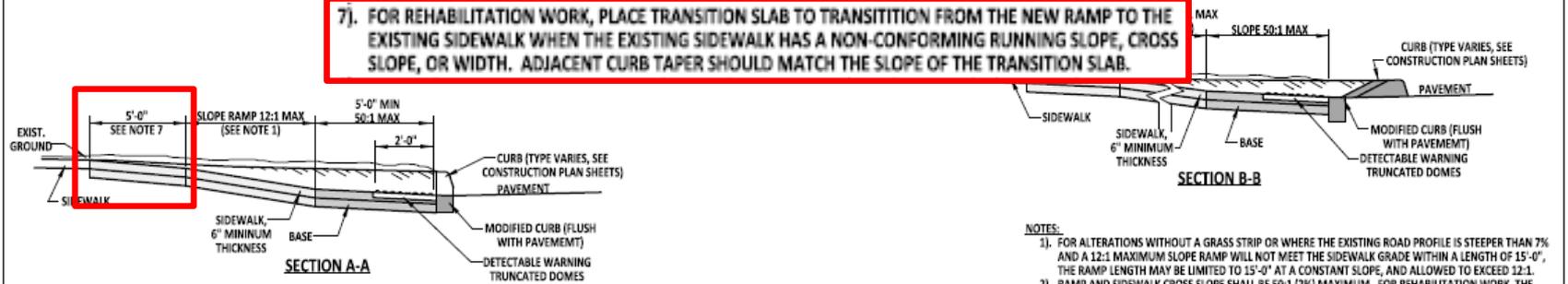
SCALE: NTS



DETECTABLE WARNING TRUNCATED DOME DETAILS

NOTES:
 A). THE AREA OF DETECTABLE WARNING TRUNCATED DOMES SHALL BE 2'-0" LONG AND THE FULL WIDTH OF THE RAMP OR DEPRESSED CURB.
 B). SEE SPECIFICATION FOR ADDITIONAL INFORMATION.

7). FOR REHABILITATION WORK, PLACE TRANSITION SLAB TO TRANSITION FROM THE NEW RAMP TO THE EXISTING SIDEWALK WHEN THE EXISTING SIDEWALK HAS A NON-CONFORMING RUNNING SLOPE, CROSS SLOPE, OR WIDTH. ADJACENT CURB TAPER SHOULD MATCH THE SLOPE OF THE TRANSITION SLAB.



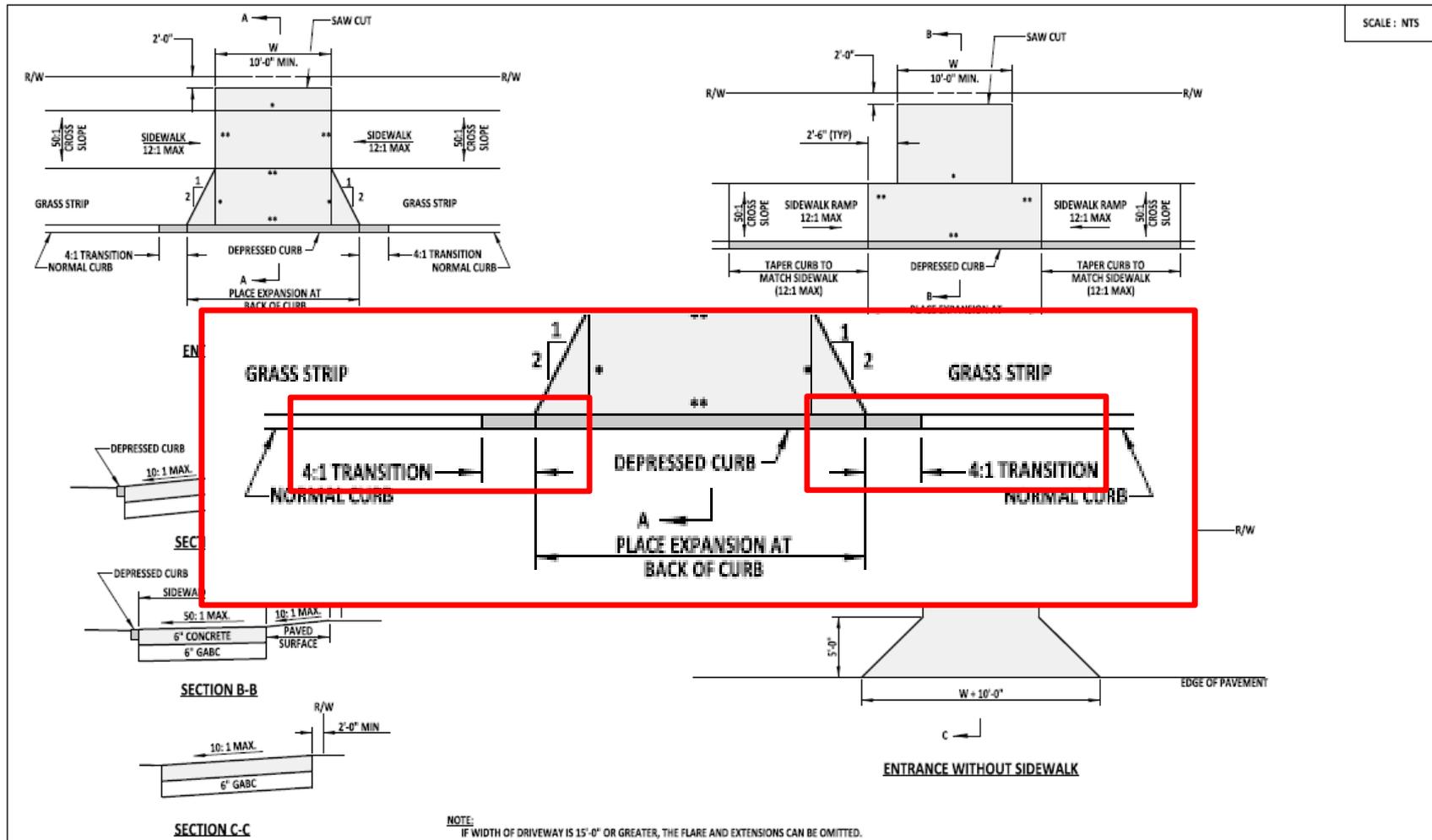
- NOTES:
- 1). FOR ALTERATIONS WITHOUT A GRASS STRIP OR WHERE THE EXISTING ROAD PROFILE IS STEEPER THAN 7% AND A 12:1 MAXIMUM SLOPE RAMP WILL NOT MEET THE SIDEWALK GRADE WITHIN A LENGTH OF 15'-0", THE RAMP LENGTH MAY BE LIMITED TO 15'-0" AT A CONSTANT SLOPE, AND ALLOWED TO EXCEED 12:1.
 - 2). RAMP AND SIDEWALK CROSS SLOPE SHALL BE 50:1 (2%) MAXIMUM. FOR REHABILITATION WORK, THE RAMP CROSS SLOPE SHALL MATCH THE SLOPE OF THE ADJACENT ROADWAY.
 - 3). IF GRADING WILL BE STEEPER THAN 6:1, THEN A TYPE 1-8 CURB OR RETAINING WALL SHOULD BE USED TO ELIMINATE THE NEED FOR THE STEEP SLOPE.
 - 4). THE MAXIMUM DIFFERENCE IN GRADE BETWEEN THE CURB RAMP OR MODIFIED CURB AT THE FLOW LINE AND THE PAVEMENT SHALL BE 13%, HOWEVER 11% IS PREFERRED. SEE DETAIL ON THIS SHEET.
 - 5). LANDING AREA SHALL BE EXTENDED 18" MIN BEYOND THE PEDESTRIAN PUSH BUTTON FOR ALL CURB RAMP TYPES. WHEN NO PEDESTRIAN PUSH BUTTON EXISTS, THE 18" EXTENSION CAN BE OMITTED.
 - 6). LANDING AREA SHALL BE DELINEATED WITH JOINTS.
 - 7). FOR REHABILITATION WORK, PLACE TRANSITION SLAB TO TRANSITION FROM THE NEW RAMP TO THE EXISTING SIDEWALK WHEN THE EXISTING SIDEWALK HAS A NON-CONFORMING RUNNING SLOPE, CROSS SLOPE, OR WIDTH. ADJACENT CURB TAPER SHOULD MATCH THE SLOPE OF THE TRANSITION SLAB.
 - 8). REFER TO THE DELAWARE MANUAL FOR UNIFORM TRAFFIC CONTROL DEVICES FOR DETAILS REGARDING THE LOCATION OF PEDESTRIAN PUSH BUTTONS.

MAXIMUM DIFFERENCE IN GRADE FOR ALL CURB RAMP TYPES
 FOR EXAMPLE, IF THE CURB RAMP AND DEPRESSED CURB SLOPE AT THE FLOW LINE (X) IS 8.1% AND THE PAVEMENT SLOPE (Y) IS 4.0%, THEN TO DETERMINE THE DIFFERENCE IN GRADE, ADD X + Y TO GET 12.1%, WHICH IS GREATER THAN THE 11% PREFERRED BUT LESS THAN THE 13% MAXIMUM.

CURB RAMP, TYPE 1
 PERPENDICULAR CURB RAMP

| | | | | | |
|--|--------------------------------|------------|-------------|-------------------|------------|
| <p>DELAWARE DEPARTMENT OF TRANSPORTATION</p> | CURB RAMP, TYPE 1 AND SECTIONS | | APPROVED | SIGNATURE ON FILE | 01/07/2013 |
| | STANDARD NO. | C-2 (2012) | SHT. | 1 | OF |
| | | | RECOMMENDED | SIGNATURE ON FILE | 12/20/2012 |

DETAIL C-3, SHEET 1 - ENTRANCES



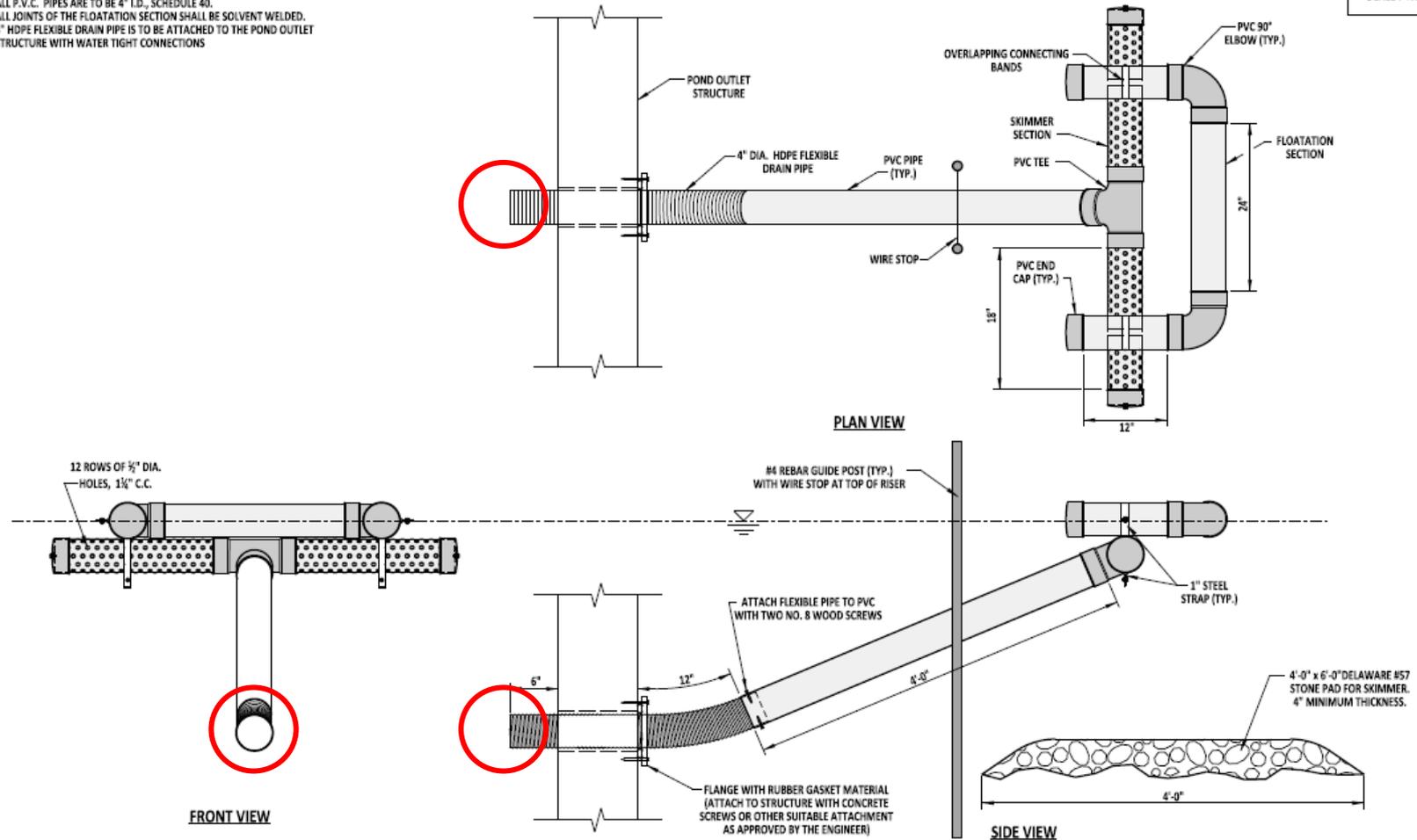
SCALE: NTS

DETAIL E-22, SHEET 1 - SKIMMER DEWATERING DEVICE

NOTES:

- 1). ALL P.V.C. PIPES ARE TO BE 4" I.D., SCHEDULE 40.
- 2). ALL JOINTS OF THE FLOATATION SECTION SHALL BE SOLVENT WELDED.
- 3). 4" HDPE FLEXIBLE DRAIN PIPE IS TO BE ATTACHED TO THE POND OUTLET STRUCTURE WITH WATER TIGHT CONNECTIONS

SCALE: NTS



DELAWARE
DEPARTMENT OF TRANSPORTATION

SKIMMER DEWATERING DEVICE

STANDARD NO. E-22 (2012) SHT. 1 OF 1

APPROVED

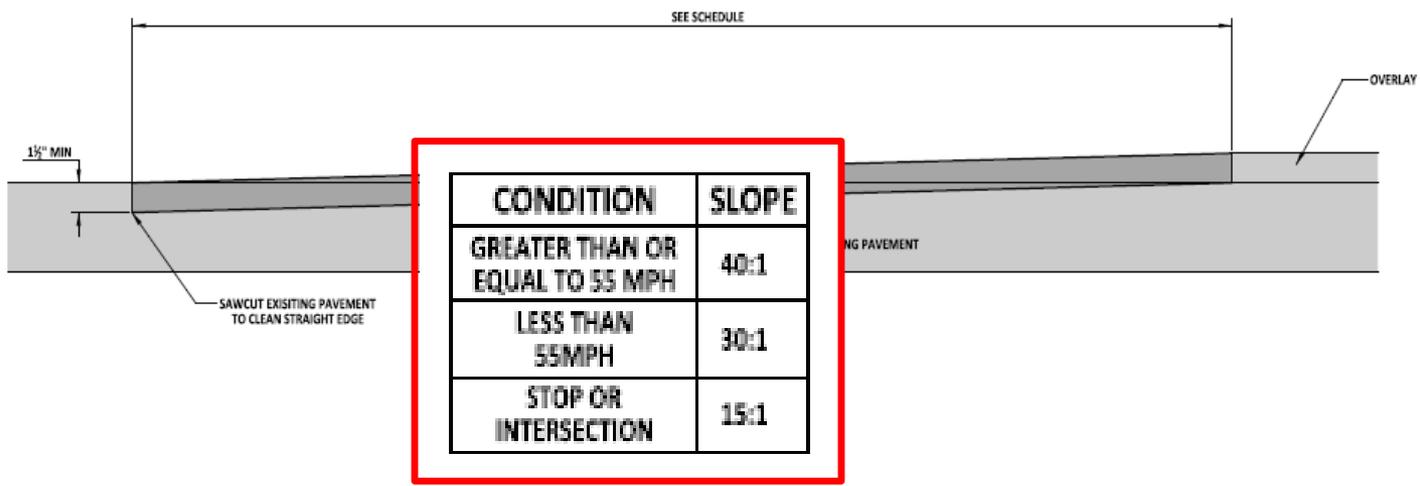
SIGNATURE ON FILE
DATE 01/07/2013

RECOMMENDED

SIGNATURE ON FILE
DATE 12/20/2012

DETAIL P-3, SHEET 1 - BUTT JOINTS

SCALE: NTS



NOTE:
THE PROFILE OF THE OVERLAY PAVING SHALL BE ADJUSTED TO ASSURE A SMOOTH TRANSITION THROUGH THE BUTT JOINT.

| CONDITION | SLOPE |
|---------------------------------|-------|
| GREATER THAN OR EQUAL TO 55 MPH | 40:1 |
| LESS THAN 55MPH | 30:1 |
| STOP OR INTERSECTION | 15:1 |

OTHER CHANGES

- General Changes
 - Removing Metric Dimensions
 - Detail C-2, All sheets - Curb Ramp Details
 - Removed Detail C-2, Sheet 3 of 4 - Elevations
 - Pertinent information was combined into remaining 3 sheets
 - Significant changes were made to the Types 3 and 4 ramps.
 - Traffic Details
 - Some minor changes to a portion of the Traffic Details. Speak with Traffic for more information.

Any questions or comments??

If you have any questions or comments in the future, please contact me by email: Billy.Sweeney@state.de.us or by telephone: (302) 760-2350